

Part B5 Controls for the Oran Park Employment Area

1.0 Introduction

1.1 Land to which this Part Applies

This part applies to the land zoned IN1 General Industrial [and B5 Business Development](#) located in the north-west portion of the precinct, as shown in **Figure 1**.

In addition to the detailed controls outlined in this Part, the general controls outlined in Part A of the Development Control Plan also apply.

1.2 Purpose of this Part

The purpose of this part is to provide a planning framework that establishes the outcomes and requirements for permissible development in the Employment Area in the Oran Park Precinct, to support the provisions within *Appendix 1 Oran Park and Turner Road Precinct Plan of State Environmental Planning Policy (Sydney Region Growth Centres) 2006*.

1.3 Structure of this Part

The Part is structured as follows:

- Section 1:** Sets out the administrative provisions of the Part
- Section 2:** Outlines the vision and development objectives
- Section 3:** Outlines the key controls and principles for the Oran Park Employment Area

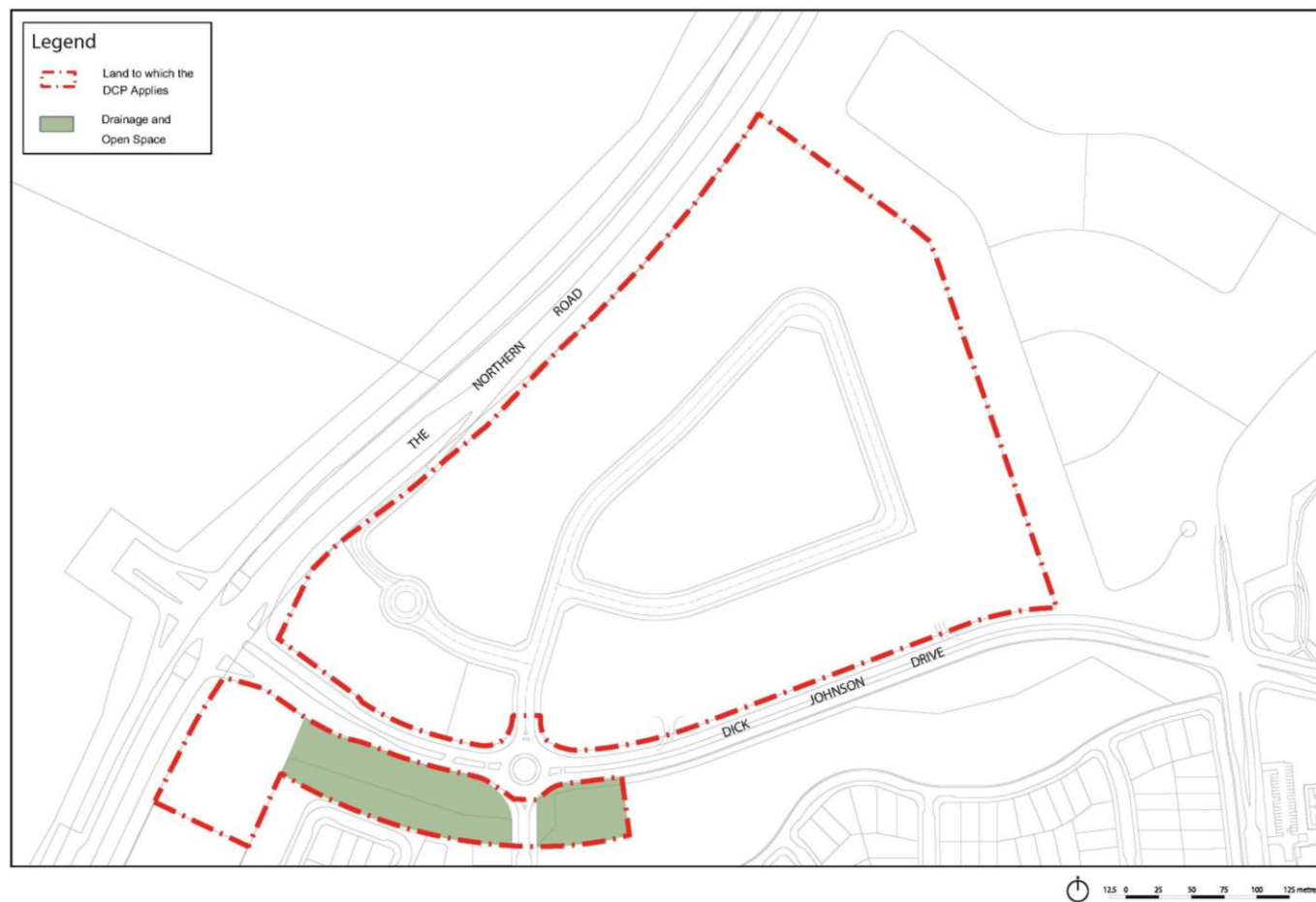


Figure 1: The Oran Park Employment Area



Figure 1: The Oran Park Employment Area

2.0 Visions and Development Objectives

2.1 Employment Area Character

Vision for the Employment Area

The Oran Park Employment Area, located in the north west portion of the Oran Park Precinct, is a dynamic employment area of quality design. It provides the opportunity for a variety of employment activities to service the Oran Park Precinct and the wider district. The Employment Area has access points from Dick Johnson Drive ~~and a dedicated left slip lane off The Northern Road and the future South Circuit road extension as well as a dedicated left slip lane off The Northern Road.~~ The left slip lane into the Employment Area will help to separate employment related and local residential related traffic movements. The Employment Area provides a simple internal road network, ensuring that traffic movements to and from the site are appropriately managed.

The Employment Area will provide a mix of employment generating activity, including business and commercial uses, general and light industrial, warehouse, specialised retail and food and drink premises and neighbourhood shops in accordance with State Environmental Planning Policy (Sydney Region Growth Centres) 2006. Development is sited and designed to provide visual interest and encourage street activation and passive surveillance particularly in land zoned B5 Business Development zone. Industrial development in land zoned IN1 General Industrial will be designed to minimise adverse impacts on nearby residential land, open space areas and other sensitive land uses. All street frontages within the Employment Area will contain quality landscaping that establishes a high standard of character and design.

Separation from nearby residential land (i.e. existing and planned residential areas south of Dick Johnson Drive and Pondicherry-Tranche 41), open space/riparian areas and other sensitive land uses in a visual and operational sense is a key design element of the Employment Area. This is managed through landscaping, limits on building heights on the perimeter of the Employment Area and locating more intensive industrial uses in the centre of the precinct.

Industrial Lands

~~General Industrial, Light Industrial, warehouse and convenience retail land uses of various size and type.~~ As shown in the Land Use Plan in **Figure 2**, ~~specific industrial~~ land uses ~~will be generally located in~~ located in either the 'Core' or 'Periphery' area where the land is zoned IN1 General Industrial. More intensive industrial land uses (where they require more specific and extensive measures to mitigate amenity impacts such as noise, odour, processing of waste) will be located in the Core area. Less intensive industrial land uses (that require standard measures to manage interface with adjoining land) will be located in the Periphery area. ~~Industrial development will operate to best practice industry standards, located and designed to avoid impacts on nearby residential lands, and open space and riparian areas. Through careful design, industrial development will reduce the visual impact of car parks and loading docks, as well facilitate safety by providing passive surveillance to the street.~~

Business Development Lands

As shown in the Land Use Plan in **Figure 2**, business ~~Buildings~~ development such as specialised retail premises will be located the 'Business Development' area, where the land is zoned B5 Business Development. Buildings fronting Dick Johnson Drive will deliver a high-quality and well-articulated built form. The combination of high quality built form, landscaping and consistent street tree plantings will deliver a visually pleasing, high-amenity gateway experience for residents, workers and visitors of Oran Park. ~~are~~ Overall, development within the Employment Area will be appropriately designed to address the street and other public domain areas, ensuring that car

parking and excessive signage does not dominate and detract from the streetscape.

2.2 Key Development Objectives

The key objectives of this Part are:

- a) To maximise opportunities for local employment within the Oran Park Precinct.
- b) To provide for an active and vibrant employment area by requiring development to address the Northern Road, Dick Johnson Drive and prominent streets.
- c) To ensure that the detailed design of the Oran Park Employment Area is undertaken in a co-ordinated manner to achieve an urban design outcome of high quality.
- d) To ensure Northern Road and Dick Johnson provide attractive, landscaped entry points to the employment area.
- e) To ensure traffic can enter and move throughout the employment area easily and safely from all entry points including The Northern Road and Dick Johnson Drive.
- f) To ensure the provision of suitable and safe pedestrian and vehicular accessibility, servicing and parking.
- g) To ensure the operation of employment activities has a minimal impact on surrounding land uses.
- h) To facilitate the flexible delivery of employment land uses in response to varying demands and requirements for land.
- i) To ensure that the impact of development upon existing site outcomes, drainage, stormwater management and water quality is adequately considered.
- j) To ensure that development promotes the principles of ecologically sustainable development.

3.0 Land Uses

Objectives

- a) To provide for a range of employment generating development with high employment levels preferred.
- b) To maximise opportunities for local employment within the Oran Park Precinct with a diverse range of employment generating development.
- c) To provide active and attractive frontages along Dick Johnson Drive.
- d) To ensure development provides an attractive frontage or is appropriately screened to The Northern Road.
- e) To minimise impacts of development on surrounding land uses by locating more intensive industrial uses within the centre of the Employment Area.
- f) To maintain the economic strength of centres by limiting the retailing of food, clothing and convenience shopping.

Controls

1. Development is to be undertaken generally in accordance with the Land Use Plan (**Figure 2**) subject to consistency with the objectives and development controls set out in this DCP and the State Environmental Planning Policy (Sydney Region Growth Centres) 2006.
2. Development should be designed to provide visual interest and achieve an attractive presentation to The Northern Road. Where an active frontage is not proposed, the

development must still provide an articulated built form and be appropriately screened from The Northern Road using high-quality landscaping.

3. Active uses that provide visual interest such as specialised retail premises should be located in the Business Development Area.* Any development that fronts or presents to Dick Johnson Drive should be well articulated and designed to activate the streetscape.
4. More intensive industrial land uses should be appropriately located within the centre of the Employment Area indicated as 'Core Area' in **Figure 2** to avoid adverse amenity impacts on residential, open space and riparian areas.
5. Development whose principal function is the storage and/or processing of goods and materials not enclosed within a building, should not be located on land which fronts a major road including The Northern Road, Dick Johnson Drive and future South Circuit road extension (North Circuit).
- 4.6. Where a vehicle body repair workshop or vehicle repair station is proposed, appropriate arrangements should be made to store all vehicles awaiting or undergoing repair, awaiting collection or otherwise involved with the development on the site of the proposed development to reduce the visual impact on the public domain. Vehicles awaiting or undergoing repair must be stored either within a building, or within a suitably screened area.
- 5.7. Any application for child centre facilities must consider the provisions of *State Environmental Planning Policy (Educational Establishments and Child Care Facilities)* 2017.

Note: For definition of specialised retail premises, please see Planning Circular PS18008.

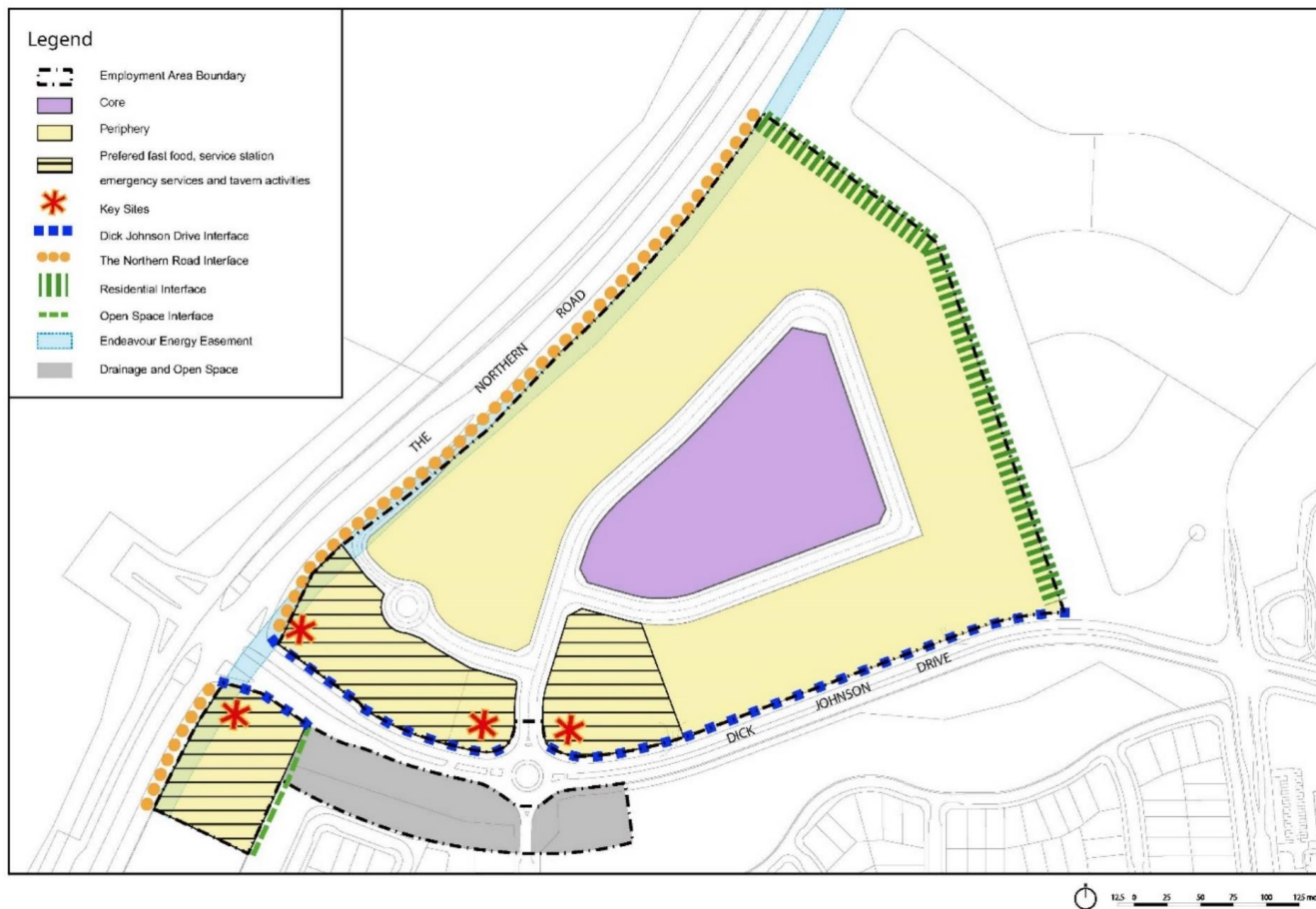


Figure 2: Land Use Plan

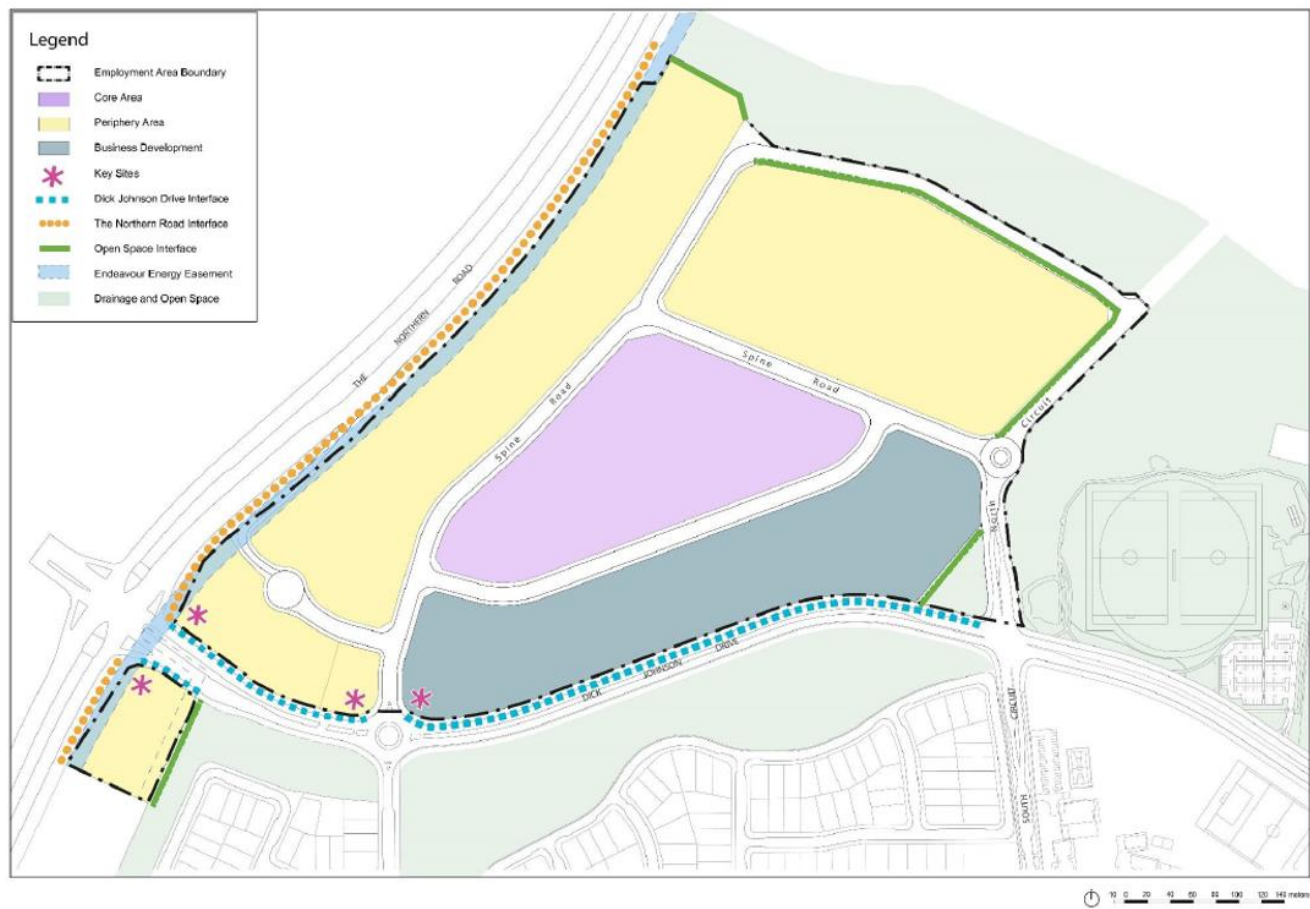


Figure 1: Land Use Plan

4.0 Subdivision

Objectives

- a) To provide a range of lot sizes to facilitate a variety of uses.
- b) To establish a subdivision layout that utilises the employment area efficiently, maximises the natural attributes of the land, appropriately addresses the street and public domain and responds to the ~~residential interface~~ key interfaces.

Controls

1. Subdivision design should be generally consistent with the road pattern shown in **Figure 3**. Where an alternative design outcome is proposed, the applicant must demonstrate consistency with the Vision and Key Development Objectives of this part of the DCP.
2. Lot sizes should be diverse to meet a range of different land uses. Irregular shaped allotments with narrow street frontages ~~must~~ should be avoided, particularly where several of these are proposed in an adjoining manner.
3. Battle-axe shaped allotments should be avoided. Where they are proposed, they must be designed in accordance with AS2890.2.
4. Subdivision applications for lots less than 2000m² (excluding any access thereto) must include supporting plans and information that demonstrate that the site can meet all other relevant development controls.
5. Lots should be orientated and aligned to encourage building design that:
 - a) faces the street to increase visual surveillance and minimise the visual impact of loading docks and long blank walls;
 - b) facilitates solar efficiency;
 - c) has frontage toward riparian corridors and open space areas.
6. Any future application for subdivision of land will require a comprehensive noise assessment of the land to be undertaken in accordance with Council's Environmental Noise Policy 2018 and section "2.4.2- Amenity Noise Levels in areas near an existing or proposed cluster of industry" of the NSW EPA's Noise Policy for Industry (NPfI). The submitted noise assessment must address the following:
 - a) apply appropriate noise limits to each lot to guide land use proposals and ensure that the Employment Area will meet relevant noise criteria as per above.
 - ~~g) consider future residential lots that are identified as 'Residential Interface' in Figure 2. For lots directly adjacent to the residential interface require a vegetation buffer and acoustic fencing made of durable materials along the common boundary as per Figure 8.~~

~~8-7.~~ Development applications for subdivision of land directly adjacent to a riparian corridor or open space (inclusive of land fronting a road adjacent to a riparian corridor or open space area) must facilitate development that:

- a) will have an articulated, aesthetic and landscaped appearance when viewed from the adjoining riparian corridor or open space land;
- b) will have minimal adverse impacts on the environment or the amenity of surrounding land;
- c) will provide opportunities for passive surveillance of the open space land, where possible;

~~9-8.~~ Where a Strata or Community Title subdivision is proposed, parking, landscaping, access areas and directory board signs must be included as common property.

5.0 Movement Network

Objectives

- a) To ensure pedestrian and traffic safety within, and to and from the Employment Area.
- b) To provide a permeable road network that facilitates movement in and through the employment area.
- c) To minimise impacts on roads including but not limited to The Northern Road and Dick Johnson Drive.
- d) To minimise impacts on surrounding residential uses and other sensitive land uses.

Controls

1. The road network should be generally consistent with the Movement Network Plan shown in **Figure 3**. Council may consider alternatives to the Plan where appropriate urban design outcomes can be demonstrated and the overall functional objectives of the network are maintained or improved.
2. Roads in the Employment Area are to be provided in accordance with **Figure 3** and designed in accordance with Council's Engineering Specifications. **Figure 4A** and **Figure 4B** provide the minimum dimensions for roads in the employment area. Roads may need to be wider where traffic management facilities (e.g. roundabouts, etc.) are determined to be required.
3. Where required by Council, a traffic report must be submitted with the development application. The report must:
 - a) demonstrate that the minimum road cross section or an alternative road cross section is appropriate for each road;
 - b) recommend appropriate traffic management facilities at intersections in the employment area.
 - c) specify the type of heavy vehicles to service the site and adequately demonstrate safe manoeuvrability of these vehicles.
4. Direct vehicular access to and from The Northern Road is not permitted except for a single, common left-in slip lane, which links into the Employment Area as detailed in **Figure 3**.
5. All road infrastructure within and accessing the Employment Area are to be designed to safely accommodate heavy vehicles.
6. Heavy vehicles are not permitted direct access to lots from Dick Johnson Drive.
7. Off street parking must be provided in accordance with 'Part 2.18.2 Off Street Car parking rates/requirements' of Camden DCP 2019. At grade parking areas must be suitably located to minimise visual impacts to the street and public domain.
8. A shared pedestrian and cycleway, and pedestrian footpaths must be provided in accordance with **Figure 3** and **Figure 4A**.
9. All roads within the Employment Area are to be designed to accommodate buses, however it is planned that future bus routes use Dick Johnson Drive.
10. Detention and treatment of stormwater runoff from roads within the Employment Area must be addressed with development applications for subdivision.

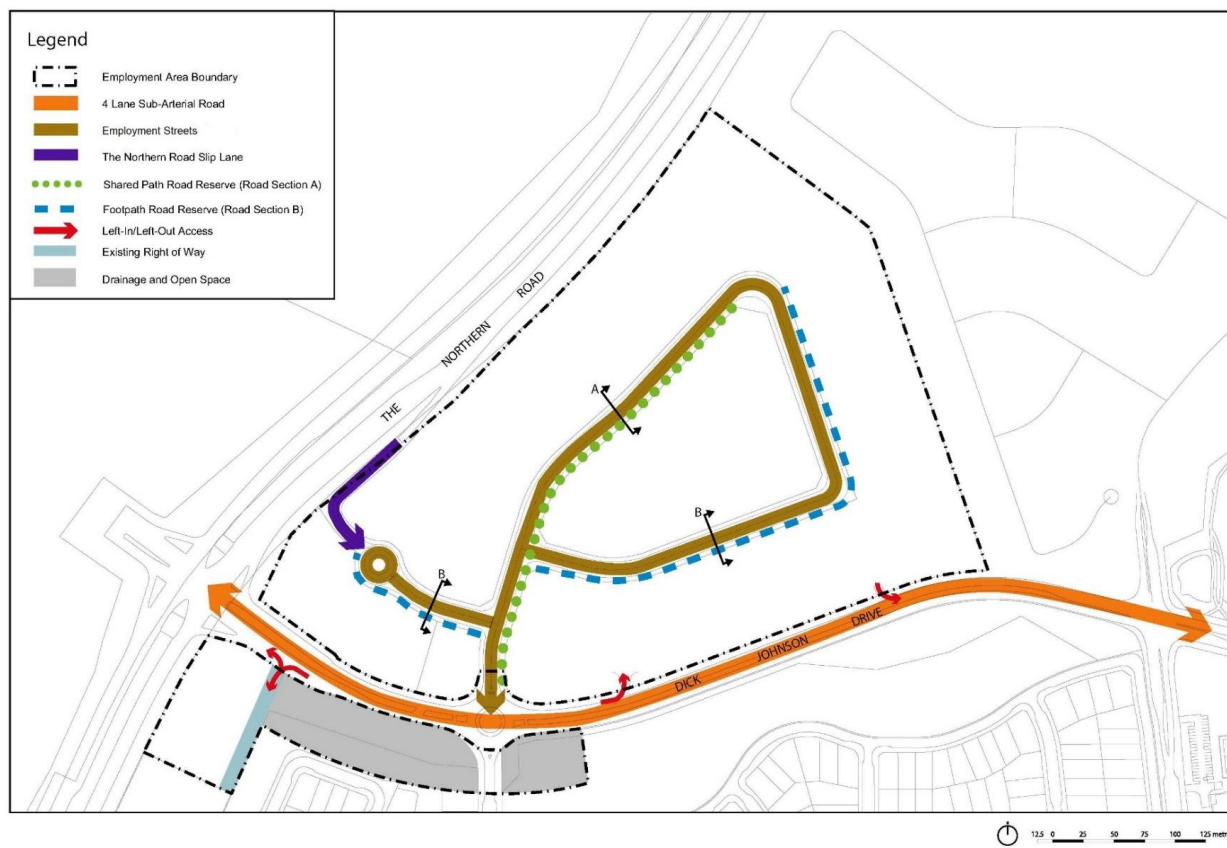


Figure 3: Movement Plan

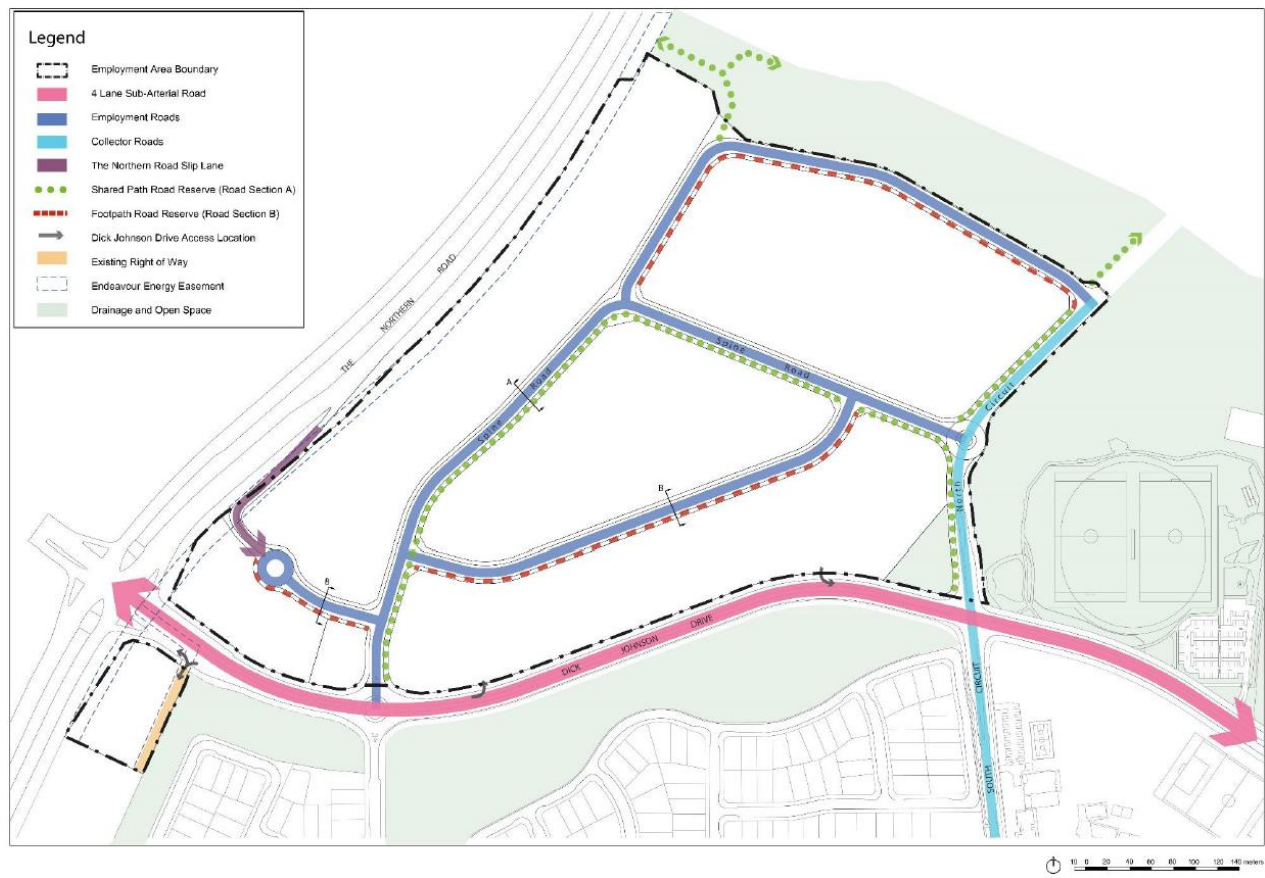


Figure 3: Movement Plan

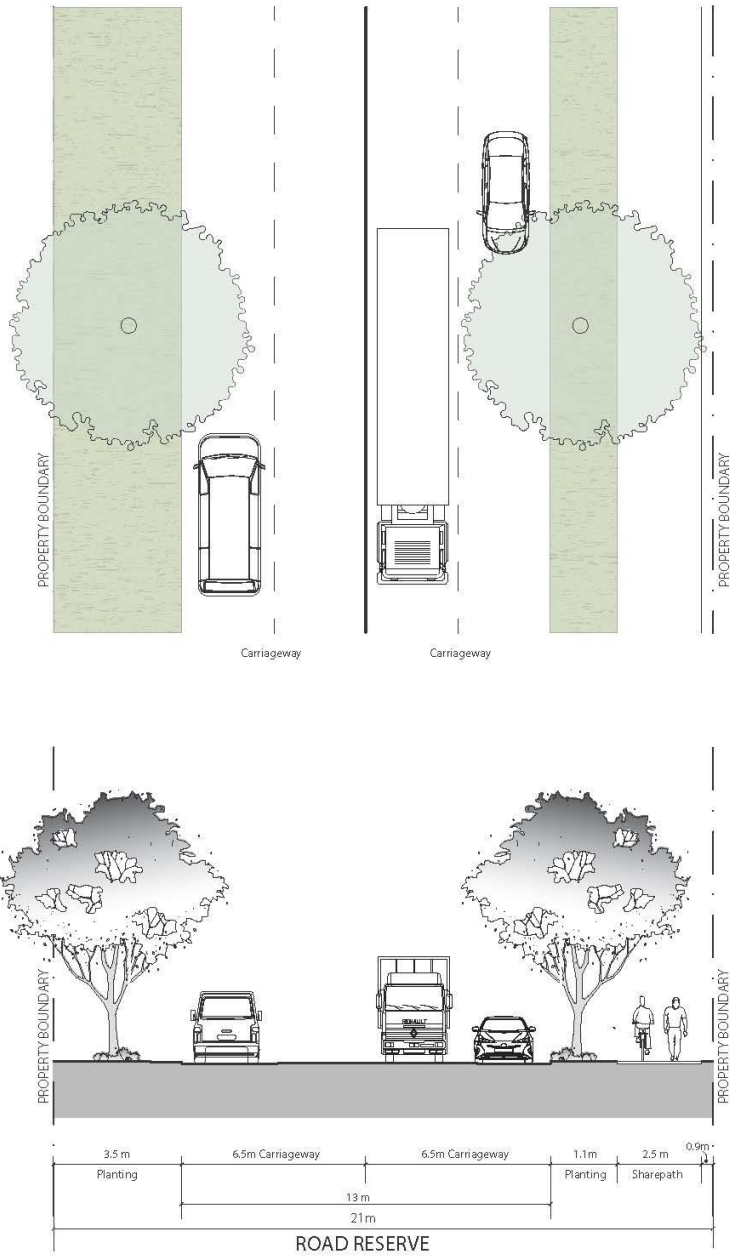


Figure 24A: Employment Land Street with Share Path (Road Section A as per Figure 3).

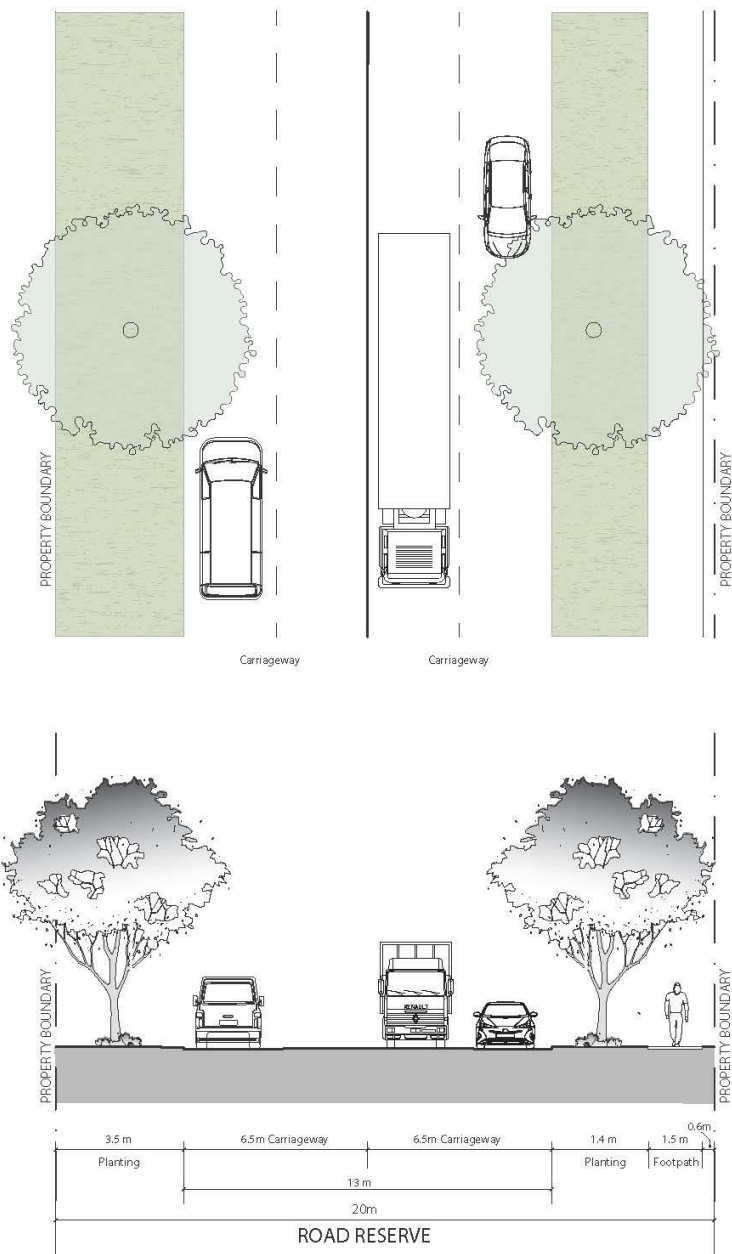


Figure 4B: Employment Land Street with Footpath (Road Section B as per Figure 3)

6.0 Site Planning

Objectives

- a) To ensure that the opportunities and constraints of the site and surrounding area are fully considered and incorporated into site design.
- b) To provide appropriate setbacks to the proposed use in consideration of site characteristics and location of the land.
- c) To achieve attractive streetscapes and quality landscaped settings for development.

Controls

1. Development must be consistent with the front setbacks shown at **Figure 6**.
2. Front setback areas are to be landscaped and not dominated by parking areas or loading facilities. Where car parking is proposed forward of the building line, the carparking must:
 - a) not encroach on the minimum front landscaping setback.
 - b) be suitably located away from building entrance points so that the building entrance remains visible from the street.
 - c) be adequately screened and broken up with suitable landscaping (e.g. shrubs) and/or fencing while maintaining view lines between the street and the building; and
 - d) designed to provide convenient, safe and comfortable pedestrian access to the building entrance.
3. Development sites must be designed to minimise the impacts of stormwater run-off on adjoining riparian corridors and demonstrate compliance with Section 8.2 Stormwater and Construction Management of Part A of this DCP.

Note: Refer to Section 6.1 Public Domain and Special Interface Areas for further site planning controls.

6.1 Public Domain and Special Interface Areas

Objectives

- a) To create quality streetscapes with high pedestrian amenity.
- b) To recognise key sites and apply specific controls to enhance their amenity and manage potential impacts, including the mitigation of opportunities for crime.
- c) To manage interface areas to reduce the impact of the employment area on nearby residential land, riparian corridors and open space areas, and other sensitive land uses.
- d) To provide high amenity break-out spaces for workers.

Controls

1. The streets in the Employment Area are to be designed and constructed to provide pedestrian safety, amenity and visual interest. Footpaths are to be provided generally in accordance with **Figure 3** and **Figure 4A** and **Figure 4B**.
- 1.2. Where development is proposed on land adjoining/adjacent to the riparian protection area identified under the State Environmental Planning Policy (Sydney Region Growth Centres) 2006, it must comply with the relevant provisions of Part B2 – Controls for Land containing a Riparian Protection Area of this DCP.
- 2.3. Landscaping should utilise predominately native, drought resistant species.

3.4. The principles of Crime Prevention Through Environmental Design (CPTED) and the Safer by Design (NSW Police) must be incorporated into the design of all development within the Employment Area.

4.5. Street tree plantings must be provided to enhance the streetscape and provide shade for pedestrians.

5.6. Development must comply with the following general design considerations set out below:

- a) Building elevations must be sited to positively address the street frontage.
- b) Long unbroken walls (i.e. walls in excess of 15 metres in length) generally must be avoided.
- c) Provide visual interest and passive surveillance by incorporating design features such as:
 - articulated building façade;
 - recessing or projecting architectural elements;
 - mixture of building materials;
 - palette of colours and finishes to enhance variation;
 - glazing to ground and additionally, to first floor levels;
 - awnings;
 - roof feature; and
 - landscaping.
- d) Car parking that is visually dominant from the street should be avoided. Where car parking is proposed forward of the building line, the carparking must:
 - not encroach on the minimum front setback required for landscaping.
 - be suitably located away from building entrance points so that the building entrance remains visible from the street.
 - be adequately screened and broken up with suitable landscaping (e.g. shrubs) and/or fencing while maintaining view lines between the street and the building.
 - be designed to be provide convenient, safe and comfortable pedestrian access to the building entrance.
- e) Provide a coordinated landscape theme that is consistent with the provisions of **Section 7 Setbacks** and **Section 11 Landscaping**.
- f) Provide a clear, articulated public entry that is visible from the street and clearly indicate entry/exit access pathways.
- g) Provide a delineated and safe pedestrian pathway to the building entrance, separate from vehicular access.
- h) Ensure that site servicing, loading facilities and waste storage are suitably located and designed such that they do not dominate the streetscape. When visible from the street, the loading dock and its doors should be:
 - setback at least 1m from the building line; and
 - be integrated with the front building façade, and comprise of neutral/recessive colours which minimise its visual impact on the street.
- i) Ensure that utilities are suitably located to the side or rear of the building where possible and designed to minimise visual impact on the public domain. Where proposed, utilities should:
 - have colour treatments compatible with the building façade; and
 - suitably screened from the street with landscaping

j) Proposed signage should be:

- minimised in terms of number, size and extent;
- generally limited to business identification signage and/or building identification signage that is integrated into the building design and/or pylon signs at entry/exit points; and
- encouraged on corner sites where business identification signage and/or building identification signs are integrated into the building façade and help to activate the primary and secondary frontage.

k) Consistent, open-style fencing is preferred. Proposed fencing must be provided in accordance with **Section 12.2 Fencing**.

6.7. All development must provide indoor or outdoor break-out spaces for the amenity of workers. If a development is to provide an outdoor amenity area, that area should:

- a) have a minimum area of 24m² with a minimum dimension of 4m;
- b) provide seating, tables, shading and adequate paving; and
- c) be located away from intrusive noise sources (e.g. loading and servicing areas and heavy machinery), dust, vibration, heat, fumes, odour or other nuisances.

7.8. Additional design considerations set out below, will apply to Special Interface Areas identified in the character statements below.

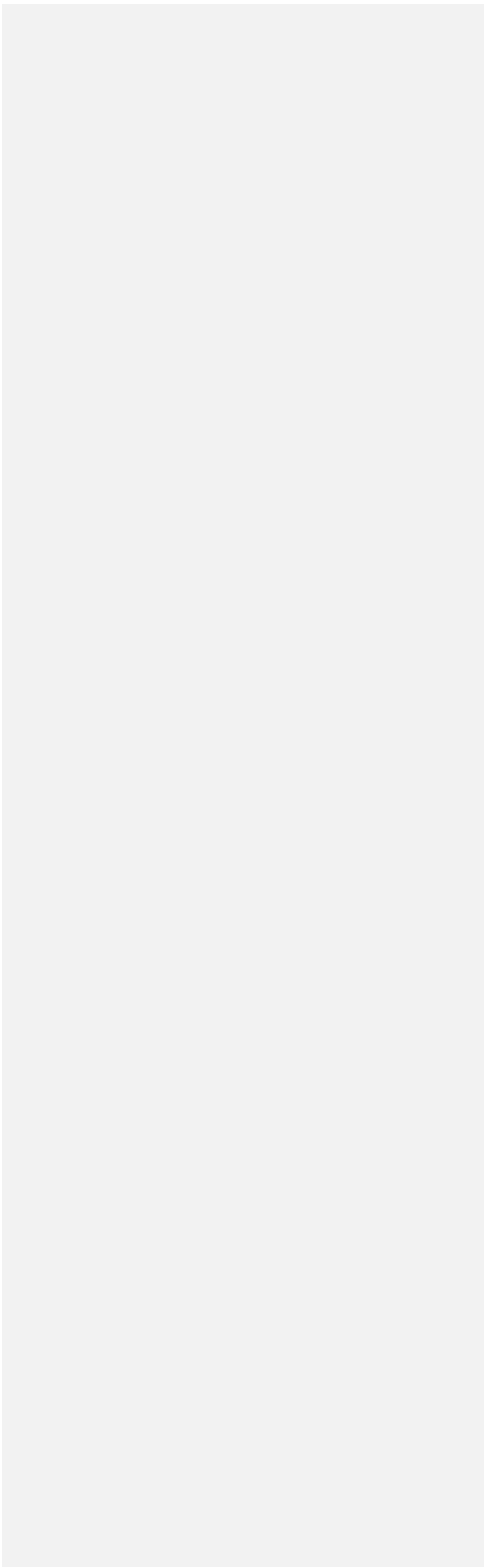
Dick Johnson Drive Interface

The Dick Johnson Drive Interface serves a main entry point for the Employment Lands and overall precinct. As a significant entry point to Oran Park, development fronting Dick Johnson Drive will be encouraged to have active uses and consistent landscaping to create a high-quality streetscape that is pedestrian-friendly. Development will be carefully designed to avoid a streetscape with visually dominant car parking, loading docks and long blank walls.

The area also interfaces with the open space and drainage area to the south side of Dick Johnson Drive. To cater for walking and cycling between the Employment Area and surrounding open space area, a shared path connection is being provided along Dick Johnson Drive and to and within the Employment Area.

- a) Avoid long expanses of blank walls (i.e. walls in excess of 15 metres in length). Where long walls are proposed, the facade should be structurally and visually articulated through the incorporation of design elements such as window glazing, mix of building materials and finishes, roof features, awnings, blade walls, colour variation.
- b) Where there is potential for side building façades to be visible from the public domain, they should be provided with variation in colour and replicate treatments from the front façade to enhance visual presentation.
- c) Provide building entries and office components close to and clearly visible from the street frontage to provide a clear visitor entry point and to activate the street frontage.
- d) Carparking must be suitably designed to reduce its visual impact on Dick Johnson Drive. Specifically,
 - carparking should be designed to provide convenient, safe and comfortable pedestrian access to the building entrance, where possible, it is preferred for carparking to be located to the side or rear of the building so that parking does not dominate the street frontage.
 - where proposed forward of the building line, carparking must be adequately screened and broken up with suitable fencing and/or landscaping (e.g. shrubs of a mature height).
- e) Business signage should be carefully incorporated into the building design.

The concept layouts below illustrate how the above design objectives could be achieved.



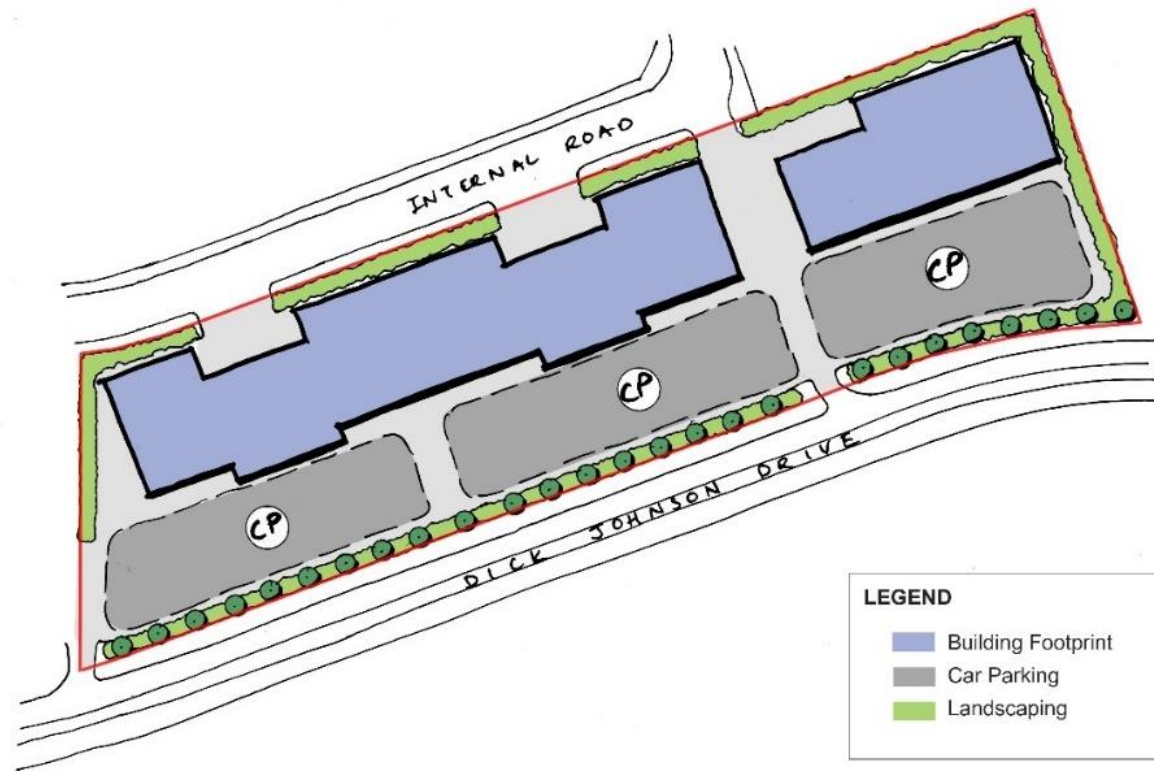


Figure 5A: Dick Johnson Drive Concept Layout 1

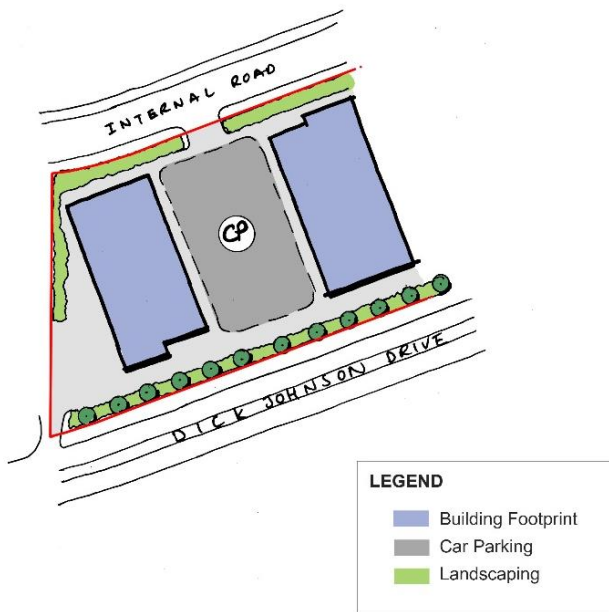


Figure 5B: Dick Johnson Drive Concept Layout 2

The Northern Road Interface

To the west of the Employment Lands is The Northern Road Interface where opposite is future residential development and the Northern Neighbourhood Centre in the north-west portion of the precinct. Development along the Northern Road Interface must be suitably designed to address the visual impact of built form on the Northern Road. This will be achieved through a wider setback from the Northern Road with suitable landscape treatment and high-quality built form that provides visual interest so as to take advantage of passing traffic.

- a) Avoid long expanses of blank walls (i.e. walls in excess of 15 metres in length). Where long walls are proposed, the facade should be structurally and visually articulated through the incorporation of design elements such as articulated second storey element, roof features, awnings or blade walls as well as colour variation.
- b) Providing landscape breaks through canopy vegetation and integrating building identification signs into the building design, should also be considered.
- c) In locations where an Endeavour Energy powerline easement is identified, landscape and fencing treatments are to be in accordance with Endeavour Energy specifications.

Open Space and Riparian Land Interface

As outlined in green in Figure 2, The Employment Area interfaces with the following open space and riparian lands including:

- riparian land to the north also known as 'Catherine Creek' which borders Pondicherry (Tranche 41);
- riparian land to the east and south also known as 'Anthony Creek';
- passive open space areas south of Dick Johnson Drive, adjacent to the substation and extending further east and co-locating with Anthony Creek; and
- the playing fields of Jack Brabham reserve directly opposite the Employment Area and adjacent to the future South Circuit road extension (North Circuit).

The Employment Area has provided pedestrian footpaths and shared paths adjacent to open space/riparian land as identified in Figure 3 including shared path links to Pondicherry (Tranche 41).

Development that interfaces with open space and/or riparian land must be suitably designed to minimise the visual and amenity impacts on these sensitive land uses, and ensure pedestrian safety and amenity. This will be achieved through compliance with the following design considerations:

- a) Avoid long expanses of blank walls (i.e. walls in excess of 15 metres in length). The facade should be structurally and visually articulated through the incorporation of design elements such as roof features, awnings or blade walls as well as colour variation. Providing landscape breaks through canopy vegetation and integrating building and/or business identification signs into the building design, should also be considered.
- b) Consistent, open-style fencing is preferred to provide passive surveillance to open space areas. Alternative solutions may be considered by Council where it can be demonstrated that the fencing is well-designed (with surface finishes incorporating a combination of colour, texture and pattern) to maintain high visual amenity to the open space area.
- c) Notwithstanding design consideration (b) above, fencing must be of a solid construction where it adjoins riparian land, and/or where Council deems it appropriate and/or necessary to provide noise attenuation and visual screening based on the nature of the development. Fencing must also be of an appropriate design (with surface finishes incorporating a combination of colour, texture and pattern) to provide visual interest.
- d) Development on land that interfaces with open space and riparian land as identified in Figure 2, must have minimal adverse impacts on the environment or the amenity of these surrounding land uses.

- e) Fencing design must consider the provision of passive surveillance to encourage pedestrian safety.

Internal Ring-road Interface

The siting and design of development along the internal ring-road interface should provide passive surveillance to the street. Site servicing and loading facilities, waste storage and other infrastructure are to be suitably located and designed to minimise visual impact on the public domain.

Key Sites

Key sites have been identified within the precinct. They will serve as main entry points and are located along the Northern Road and Dick Johnson Drive as identified in **Figure 2**.

- a) Development on these key sites must be appropriately sited and suitably designed to ensure that presentation to both road frontages is achieved.
- b) Building identification signs that are integrated into the building façade should be encouraged on corner sites to help activate both the primary and secondary frontage.

6.2 Topography Cut and Fill

The topography of Oran Park provides local and distant vistas from public and private domains. The topography of the Oran Park Employment Area gently falls in a west to east direction from The Northern Road. Industrial development should consider the topography of the land so that building and site design ensure minimal impact on surrounding development and the public domain.

Objectives

- a) To protect and enhance the aesthetic quality of the area by controlling the form, bulk and scale of land forming operations.
- b) To ensure the dimensions of building footprints are appropriately sized and sited to minimise the extent of cut and fill.
- c) To ensure that the amenity of adjoining open space and drainage areas and other sensitive land uses is not adversely affected by any land forming operations.
- d) To promote attractive streetscapes where landscaped retaining walls are visible from the street.

Controls

1. Development applications must illustrate where it is necessary to cut and/or fill and provide justification for the proposed changes to the land levels.
2. Retaining walls visible from the street and public domain should be a maximum height of 1m before incorporating a landscape step and all earthwork batters are to be landscaped to mitigate adverse visual impacts.

7.0 Setbacks

Objectives

- a) To ensure buildings are of an appropriate bulk and scale when viewed from the street and public domain.
- b) To ensure setbacks are appropriate for the proposed use and location of the site.
- c) To enable the integration of built and landscape elements to create an attractive, visually consistent streetscape.
- d) To ensure that carparks, site servicing, loading facilities and waste storage do not dominate the streetscape.

Controls

1. Minimum building setbacks are to be in accordance with **Figure 6**.
 - a) Notwithstanding clause (1) above, no building is permitted within the Endeavour Energy powerline easement located adjacent to The Northern Road.
 - b) In the event that any identified easement is removed, the building setbacks identified in **Figure 6** apply.
 - c) Where a 2m setback is applied on the Building Setback Plan in **Figure 6**, an active street frontage must be provided.
1. Outdoor amenity areas and small ancillary uses, such as cafe shade structures and seating areas for employees and visitors, must be located behind the landscaped setback to maintain visual and acoustic privacy.
2. Where the site is a corner lot that also addresses Dick Johnson Drive, the primary and secondary frontage must be treated as that facing Dick Johnson Drive and apply the development controls accordingly.
3. Front setbacks are to be landscaped in accordance with **Section 11 'Landscaping'**. Where parking is proposed forward of the building line, the parking area is not permitted within the landscaping setback. Carparking must be integrated with landscaping to provide convenient and safe access to the building entrance.
- ~~3.~~ Rear and side setbacks are zero where the site is directly adjoined by other industrial uses. Where a zero setback is proposed, the design and construction of buildings are to be in accordance with Building Code of Australia and relevant Australian Standards. Rear and side setbacks may be required by Council and considered on merit depending on the nature of adjoining development.
- ~~4.~~ ~~A 5m vegetated buffer must be provided along the residential and employment area interface within the rear portion of the industrial allotments in accordance with **Figure 6** and **8**.~~

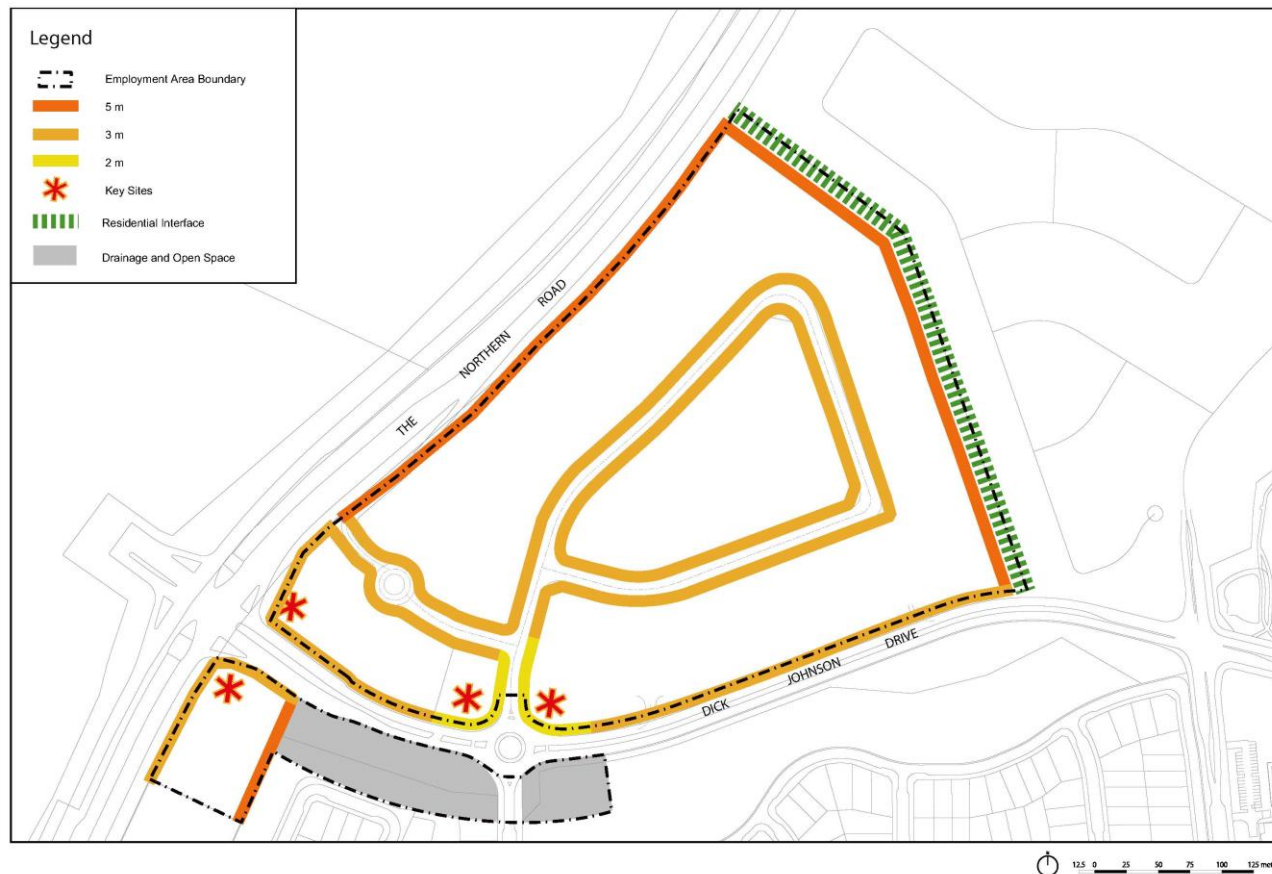


Figure 6: Building Setback

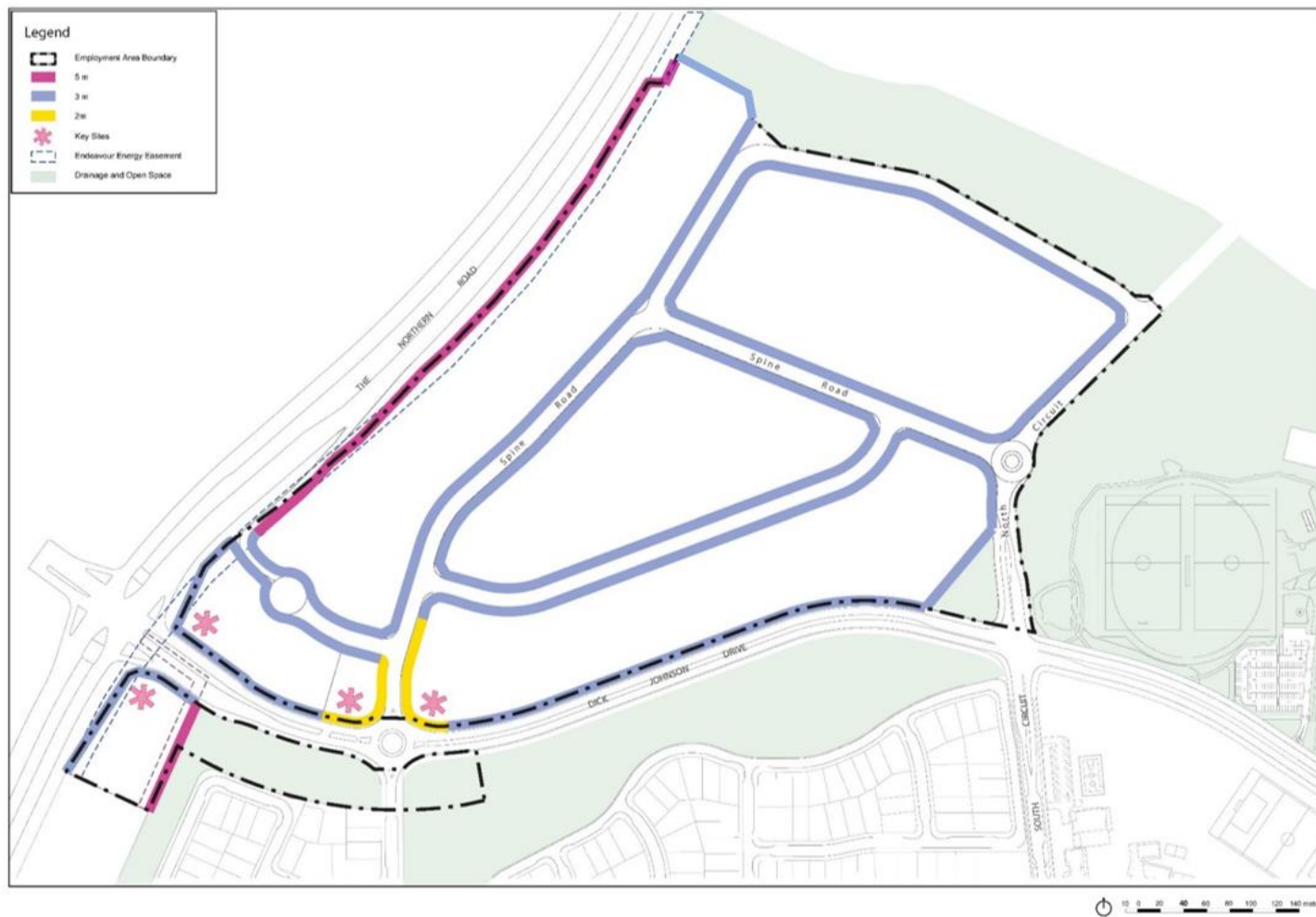


Figure 6: Minimum Building Setback

8.0 Height of Buildings

Objectives

- a) To ensure that building height does not adversely affect surrounding land uses or scenic amenity.
- b) To ~~consider-balance the~~ intended use, visual bulk, human scale and appropriate scale of development, while ensuring that land is not underutilised.
- ~~b)c)~~ To reduce the visual impact of built form on roads including but not limited to Dick Johnson Drive and The Northern Road.

Controls

1. Building heights are to be in accordance with **Figures 2 and 7**.
 - ~~a) In the area marked 'periphery' area' and 'core area' in Figure 7, a maximum building height of 15m is permitted for development fronting The Northern Road and Dick Johnson Drive.Δ~~
 - ~~b) In the area marked 'residential interface', the height of industrial buildings adjacent to residential dwellings (identified as 'Residential Interface') must not exceed 9.5m for a distance of 10.5m from the rear boundary in accordance with Figure 8.~~
 - ~~c)a)~~ In the area marked "core area", the maximum building height of 15m applies for all development. Building heights greater than 15m may only be considered in the core area, where higher industrial structures are proposed.

Note:

~~Δ~~ Heights are limited to a maximum 15m along The Northern Road and roads fronting Dick Johnson Road ('East-West road'), in accordance with clause 4.3(5) of Appendix 1 Oran Park and Turner Road Precinct Plan of State Environmental Planning Policy (Sydney Region Growth Centres) 2006.

*For the purpose of building height provisions only, the 'Periphery Area' highlighted in yellow in Figure 7 also incorporates the Business Development Land identified in Figure 2. A maximum building height of 15m generally applies to the overall Employment Area.

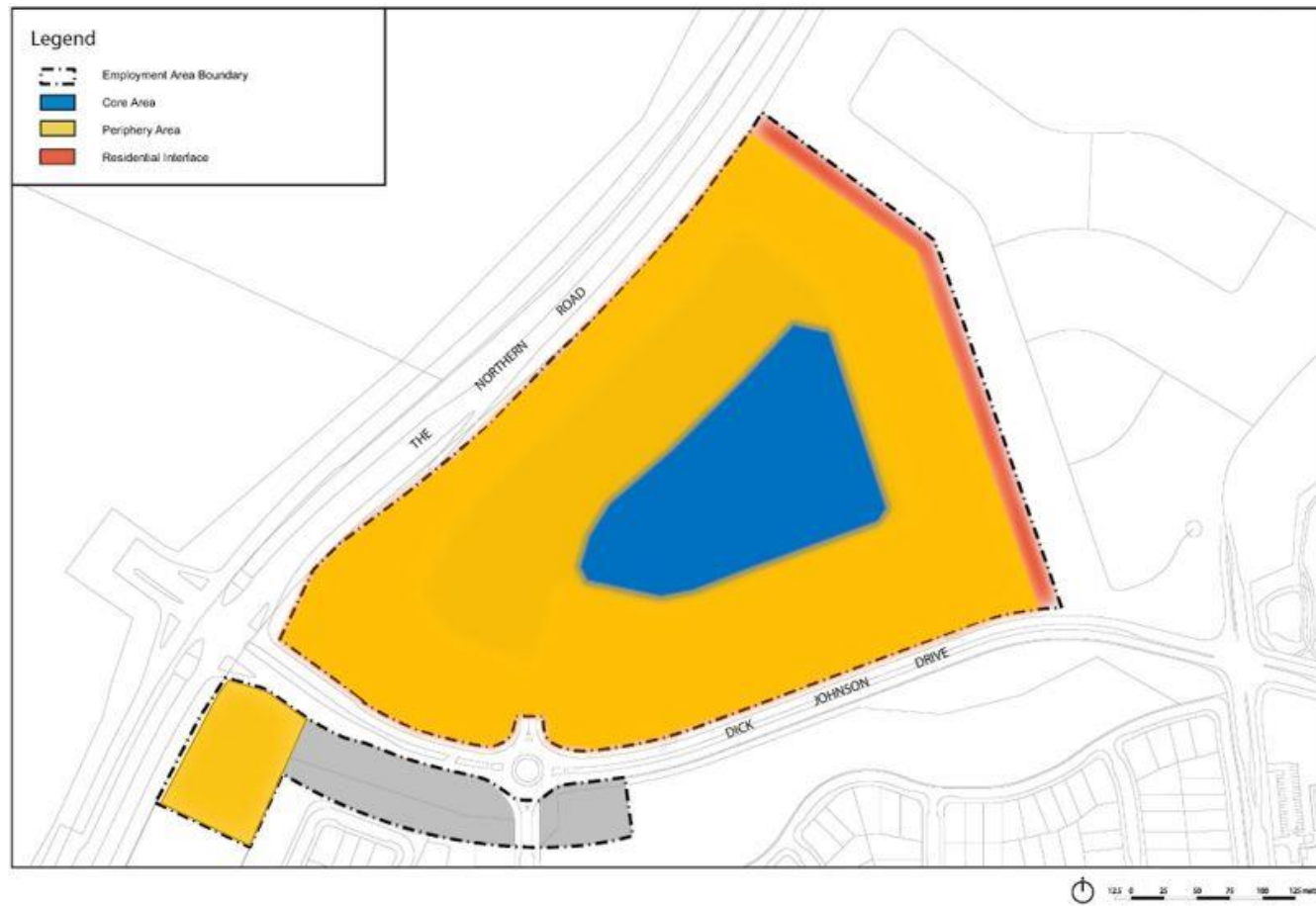


Figure 7: Height Transition



Figure 7: Height Transition

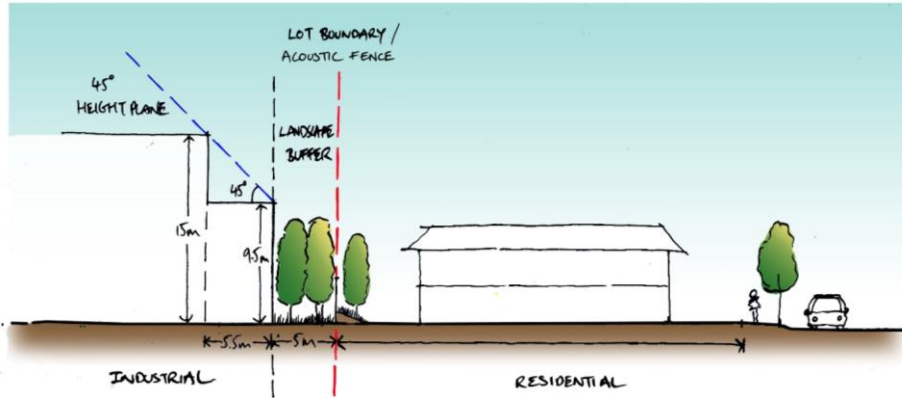


Figure 8: Employment Area and Residential Interface

Note: 5m Landscape buffer to be located within the rear portion of industrial lots.

9.0 Overshadowing

Objectives

- a) To minimise overshadowing of adjacent open space areas.
- b) To ensure satisfactory solar access to public and private spaces within the Employment Lands.

Controls

1. Any Development Application for the construction or alteration of buildings adjoining open space and drainage areas is required to submit detailed Shadow Diagrams indicating shadows cast by buildings, roof overhangs and fences for 9am, 12pm and 3pm on Summer and Winter Solstice (21st December and 21st of June respectively) inclusive of consideration of the land gradient.
2. Development should provide adequate solar access to outdoor amenity areas.
3. Development should minimise impact of overshadowing on neighbouring solar panels.

10.0 Building Design

10.1 Building Form and Materials

Objectives

- a) To ensure that materials, architectural features and the built form enhance the desired character and aesthetic of the area, streetscape and building elevations.
- b) To encourage the use of durable, non-toxic, low embodied energy, quality materials and finishes to minimise environmental impacts and ensure thermal comfort and amenity of the building.
- c) To encourage use of materials appropriate to the solar, climatic and other local conditions.
- d) To ensure a mix of materials and finishes that act to reduce the bulk, scale and mass of large buildings.
- e) To avoid large blank walls, particularly those visible from the public domain and streetscape.

Controls

1. A mix of materials, colours and architectural features must be used. A schedule of materials and colour palettes must be submitted with the development application.
2. The scale and massing of buildings should reinforce the urban design character of the precinct. Building scale and massing should generally be consistent within a streetscape.
3. The office component of any development must be incorporated into the overall design of the building and generally located along the primary street frontage.
4. The built form and architecture of development on 'Key Sites' identified in **Figure 2** must enhance its location and positively respond to and emphasise the corner.
5. Appropriate screening to suit the architectural character of the building and surrounds is required to screen plant equipment, storage, rubbish disposal and related uses in all areas.
6. Visible rooftop structures (including plant rooms, air-conditioning and ventilation systems) where visible from the public domain, **must be** incorporated into the design of the building to create an integrated appearance.
7. Blank building facades facing the primary street frontage are not permitted.
8. Design should consider and reflect passive design principles including thermal mass, solar screening, solar access/ shading to outdoor amenities, insulation, ventilation, where possible.
9. Use of metal cladding:
 - a) Use of metal cladding is discouraged on front elevations unless it can be satisfactorily demonstrated that it forms part of an architectural design solution in association with masonry, glass and other high quality materials.
 - b) Where a side or rear elevation is visible from the public domain, the use of metal cladding must comprise no more than 50% of that wall's building material.
10. Materials that are likely to contribute to poor internal air quality such as those generating formaldehyde or those that may create a breathing hazard in the case of fire (e.g. polyurethane) should be avoided.
- 11.** Glazing should not exceed 20% reflectivity.

10.2 Bulk and Scale - Large Floor Plate Development

Objectives

- a) To encourage large floor plate development that is consistent with the surrounding context.

Controls

1. Where development with a floor plate greater than 4,000m² is proposed, the following controls must be taken into consideration:
 - a) Long building façade walls should be adequately articulated and are to be less than 15m length and 5m height.
 - b) Large format signage and branding across entire elevations or window displays is not allowed.
 - c) Activation of at least 50% of the building frontage with a generous, articulated and identifiable building entrance, display windows and human scale, built form elements including shade structures, awnings, outdoor amenity spaces, landscaping, canteens and cafes is preferred.
 - d) Incorporate architectural detail and interest at visually prominent building locations such as entrances, lower level front facades, roof tops, visible corners and at the terminations of street vistas.
 - e) Development must minimise the impacts of overlooking, overshadowing, noise and lighting on adjoining land uses, and provide appropriate landscape buffers and visual screening treatments to mitigate impacts. This may include vegetation on raised mounds and/or feature acoustic walls.
 - f) Where practical, external loading docks are to be located at the rear of buildings. Detrimental amenity impacts on open space areas and other surrounding land uses must be avoided.

11.0 Landscaping

Objectives

- a) To provide consistent, quality landscaping to the streetscape and locality.
- b) To integrate a high standard of visual amenity and character into the development.
- c) To improve workplace and outdoor amenity for employees and visitors.
- d) To reduce heat island effects from hard surfaces and to improve the energy performance of buildings and the microclimate.
- e) To encourage water sensitive urban design that reduces stormwater runoff by allowing natural infiltration into landscaping.

Controls

1. A concept landscape plan must be submitted with development applications. Proposed removal of trees or vegetation must be identified on the plan.
2. Proposed landscaping must demonstrate the provision of plantings that assist in creating a human scale, reducing visual bulk to the streetscape and/or within the site and offering amenity.
3. For lots with a single or dual frontage:
 - a) setback areas facing the street must be provided with a landscape buffer with a minimum depth of 3m when boundary fencing is proposed to the street;
 - b) the minimum depth of the buffer can be reduced to 2m when no boundary fencing is proposed to the street; and
 - c) adequate and quality landscape treatment for the landscape buffer must be demonstrated through the concept landscape plan and must be maintained for the life of the development.
4. For corner lots:
 - a) setback areas facing the primary street are to be provide with a landscape buffer with a minimum depth of 3m when boundary fencing is proposed to the street or minimum depth of 2m when no boundary fencing is proposed;
 - b) adequate and quality landscape treatment for the landscape buffer must be demonstrated through the concept landscape plan and must be maintained for the life of the development; and
 - c) reduction of the minimum landscape buffer for setback areas may only be considered in exceptional circumstances (i.e. where site characteristics such as easements affect setbacks) and in these cases, it must be adequately demonstrated that:
 - the buffer depth at any point will be no less than 1m;
 - adequate and high-quality landscape treatment (a coherent landscape theme incorporating a mix of trees, hedges or hedgerows should be provided);
 - plantings will be a mature height of at least 1m at the time of planting;
 - the secondary building façade is sited and articulated to positively address the street; and
 - overall visual bulk and scale is reduced to positively respond to human scale.

5. Notwithstanding controls (3) and (4), lots fronting roads must generally provide dense landscaping (a coherent landscape theme incorporating a mix of trees, hedges and hedgerows) to adequately soften the built form.

5.6. Proposed landscaping forward of the building line should maintain open view lines between the street and the building use.

6.—Carparks within the Employment Area must provide for the planting of trees and shrubs in accordance with Section 2.18.3 of Car parking design criteria. In particular, car parking areas within the Business Development Area (identified in **Figure 2**) must:

- provide a 2.5m wide landscape bay between every 6-8 car parking spaces;
- provide a minimum 1m landscaping strip at the end of parking aisles;
- be landscaped in accordance with Figure 2-12 'Design features of carpark' of Section 2.18.3 of this DCP such that mature tree clusters are located at highly visible corners; and
- ensure that landscaping does not impede sightlines of drivers.

7.8. Native and low water usage plant species are preferred.

8.9. Landscaping along The Northern Road interface and within the Endeavour Energy easement must be in accordance with Endeavour Energy specifications.



Figure 8: Effective Site Landscaping



Figure 9: Inappropriate landscaping

12.0 Retail Uses

Objectives

- To provide services and small-scale retail that provides for the day to day needs of the local industrial workforce and community.
- To permit the display or sale of goods manufactured on site.
- To maintain and support the viability of existing retail centres.
- To ensure industrial land is primarily used for industrial purposes.

Controls

- Permitted retail uses under the Growth Centres SEPP will be considered by Council if it can be demonstrated that the retail use:
 - services the day to day needs of people who live and work in the local area;
 - is ancillary to permitted uses on the same allotment; and
 - does not compromise the retail primacy of local, strategic and district centres.

2. The maximum floor area of the proposed retail use (e.g. neighbourhood shops and industrial retail outlets where permitted with consent) must comply with clause 5.4 of the Sydney Region Growth Centres SEPP.

13.0 Fencing

Objectives

- a) To encourage an attractive and consistent theme of fencing within the Employment Area.
- b) To ensure fences of high quality that minimise visual impact on the streetscape, integrate with landscaping and enable passive surveillance to the street.
- c) To ensure fencing that provides acoustic and visual privacy where the Employment Lands adjoin ~~with residential areas and~~ public open space and other sensitive land uses.

Controls

1. Where fencing is proposed forward of the building line to a street it should be:
 - a) black open-style steel palisade fencing with a maximum height of 2.4m, measured from ground level (finished);
 - b) sited so it does not impede sight lines for drivers.

Details of fencing are to be provided at the development application/complying development certificate stage.

2. Fencing along The Northern Road interface adjacent to the Endeavour Energy easement, must be consistent in height along the length of The Northern Road and in accordance with Endeavour Energy specifications.
3. Notwithstanding controls (1) & (2) above, alternative fencing may be permitted where noise attenuation is required for neighbouring residential development and open space areas. Alternative fencing must be appropriately designed and treated and/or softened with suitable landscaping to provide visual and acoustic amenity to surrounding land uses.
4. For all other roads (other than The Northern Road) ~~→~~, where fencing is proposed forward of the building line, it must be located either:
 - a) Along the site boundary to the street, with a suitably landscaped buffer of at least 3m behind the fence line; or
 - b) Setback 3m from the site boundary with suitable landscaping within the 3m setback (i.e. between the lot boundary and the fence line);

5. Gates are to be consistent with the adjacent style of fencing and must be designed to open inwards.

6. Fencing adjacent to open space and riparian corridors must be:

- a) open-style fencing to provide passive surveillance of open space/riparian areas; or
- b) of a solid construction where Council deems it appropriate and/or necessary to provide noise attenuation and visual screening of development notwithstanding control 6(a) above; and
- 5-c) appropriately designed ensuring that surface finishes consider a combination of colour, texture and pattern to provide visual amenity and interest to the open space/riparian corridor.

~~Fencing adjacent to residential lots, should comprise of durable materials and provide acoustic and visual privacy.~~

14.0 Employment Operations

Objectives

- a) To provide appropriate levels and design of access, parking and loading facilities.
- b) To ensure that the operation of employment activities does not have a detrimental impact on the amenity of the surrounding residential land and open space areas.

Controls

1. Access, parking and loading to be in accordance with 'Part 2.18.2 Off Street Car parking rates/requirements' of Camden DCP 2019.
2. Development applications must provide the following details:
 - a) proposed hours of operation;
 - b) number and timing of deliveries expected per day;
 - c) nature, frequency and routes of heavy vehicles expected to access the premises;
 - d) nature of machinery proposed to be operated at the premises, including noise levels generated and noise attenuation measures proposed to be implemented; and
 - e) proposed locations and hours of operation of external light sources and the extent of light spillage outside of the new subject property.
3. Operations including heavy vehicle routes and loading/unloading times must consider and minimise impacts on the amenity of nearby residential land.
4. Appropriate measures must be taken to ensure that lighting does not create a nuisance to nearby residential land. Where deemed necessary by Council, Council may require a lighting mitigation strategy to be submitted with a development application.
5. Appropriate measures must be taken to ensure that development minimises odour/ air-pollution impacts on surrounding land uses.

15.0 Outdoor Storage

Objectives

- a) To ensure that goods, materials and equipment which are stored outside buildings do not impose any adverse environmental impacts.
- b) To ensure that storage areas are consistent with the overall building design and do not detract from the amenity and appearance of the streetscape, public domain or the broader community.
- c) To ensure that storage areas are adequately screened from the street and public domain.

Controls

- 1. External storage of goods, materials and equipment such as garbage bins, is not permitted when visible from the public domain.
- 1.2. The storage of plant, equipment, goods and other materials must be suitably screened.
- 2.3. Waste collection and outdoor storage areas must be located behind the building line, be screened with landscaping and fencing and have a sealed ground surface.
- 3.4. Development Applications must indicate:
 - a) the types of goods, materials and equipment to be stored externally;
 - b) the dimensions of the outdoor storage area; and
 - c) details of screening.

16.0 Acoustics

Objectives

- a) To encourage a precinct approach to assessing noise impacts in the Employment Area and facilitate equitable sharing of noise levels amongst all operators in the precinct.
- b) To enable the noise impacts of the Employment Area precinct to be managed over time.
- c) To ensure that there are no unreasonable noise impacts for open space areas and other sensitive land uses adjoining or nearby the Employment Area.

Controls

1. Any future application for subdivision of land will require a comprehensive noise assessment of the land to be undertaken in accordance with Council's Environmental Noise Policy 2018 and section "2.4.2- Amenity Noise Levels in areas near an existing or proposed cluster of industry" of the NSW EPA's Noise Policy for Industry (NPfI). The assessment must determine the operational noise limits applicable to each new lot measured at nearby sensitive receivers. The comprehensive noise assessment report prepared at subdivision must be included on the s88B instrument attached to the lots.
2. Development applications proposed on lot/s where operational noise limits and individual project noise levels have already been determined (and accepted by the Consent Authority), must submit with their application written certification from a qualified acoustic consultant. The written certification must confirm or demonstrate that the operation of the proposed development will comply with relevant noise levels.
3. Development must demonstrate to the satisfaction of Council, that sufficient and appropriate measures have been taken to minimise adverse noise impacts on surrounding land uses and/or nearby sensitive noise receivers.
- 2.4. Noise emitting activities, such as loading docks should be suitably located and designed to minimise adverse impacts on nearby residential areas (i.e. residential areas south of Dick Johnson Drive and Pondicherry – Tranche 41).